

Public notice

College Road and Crystal Palace Parade junction improvements

*The London Borough of Southwark (Prescribed routes) (Crystal Palace Parade) (No. 2) Traffic Order 20***

*The London Borough of Southwark (Bus priority) (Crystal Palace Parade) (No. 2) Order 20***

*The London Borough of Southwark (Waiting and loading restrictions) (Amendment No. *) Order 20***

1. Southwark Council hereby GIVES NOTICE that it proposes to make the above orders under the Road Traffic Regulation Act 1984¹, as amended.
2. The effect of the orders would be:-
 - (a) in COLLEGE ROAD:
 - (i) to prohibit south-eastbound vehicles from undertaking a 'U-turn' manoeuvre at the south-eastern end of the traffic island constructed at its junction with Crystal Palace Parade; and
 - (ii) to introduce 'at any time' waiting restrictions on both sides at and on the approach to its junction with Crystal Palace Parade;
 - (b) in CRYSTAL PALACE PARADE:
 - (i) to introduce a compulsory left turn lane for north-eastbound vehicles on the north-west side (nearside) of the north-eastbound carriageway on the approach to its junction with College Road;
 - (ii) to introduce an 'at any time' north-eastbound bus lane in the centre of the north-eastbound carriageway on the approach to its junction with College Road, for the use of buses, pedal cycles and taxis proceeding ahead into Crystal Palace Parade only;
 - (iii) to introduce an 'ahead only' north-eastbound traffic lane on the south-east side (offside) of the north-eastbound carriageway, for the use of any vehicle proceeding ahead into Crystal Palace Parade;
 - (iv) to prohibit vehicles using either the north-eastbound 'bus lane' or north-eastbound 'any vehicle lane' referred to above from turning right into the vehicular access to the Crystal Palace transmitting station ('Arqiva Crystal Palace');
 - (v) to introduce 'at any time' waiting restrictions on the north-west side between the boundary of the London Borough of Bromley and its junction with College Road; and
 - (vi) to formalise an existing 'at any time' north-eastbound bus lane on the north-west side between the boundary of the London Borough of Bromley and a point 80 metres south-west of its junction with College Road.
3. For more information contact Donovan Johnson of the council's Highways – Transport Projects team Highways@southwark.gov.uk.
4. Copies of this notice, the proposed orders, a statement of the council's reasons for making the orders, and a plan of the proposals may be found online at www.southwark.gov.uk/trafficorders, paper copies may be obtained from or viewed at Highways, Environment and Leisure, 3rd floor hub 2, 160 Tooley Street, London SE1 2QH. Email traffic.orders@southwark.gov.uk (or call 020 7525 3497) for details.
5. Anyone wishing to object to or make any other representations regarding the proposal, may use the form labelled 'Parking - Road traffic and highway schemes - responding to statutory consultation notices' at www.southwark.gov.uk/statutoryconsultationnotices or send a statement in writing to: the Traffic Orders Officer, Highways, Southwark Council, Environment and Leisure, P.O. Box 64529, London SE1P 5LX or by e-mail to traffic.orders@southwark.gov.uk quoting reference 'TMO1920-038 CPP College Rd by 12 March 2020. Please note that if you wish to object to this proposal you must state the grounds on which your objection is made.
6. When making an objection or representation, please be aware that this may be communicated to other people who may be affected. Information provided in response to this consultation, including in some circumstances personal information, may also be subject to publication or disclosure under the requirements of current access to information legislation.

Dated 20 February 2020

Nicky Costin - Parking and Network Management Business Unit Manager, Regulatory Services

¹ 1984 c.27

Statement of reasons

College Road and Crystal Palace Parade junction improvements

*The London Borough of Southwark (Prescribed routes) (Crystal Palace Parade) (No. 2) Traffic Order 20***

*The London Borough of Southwark (Bus priority) (Crystal Palace Parade) (No. 2) Order 20***

*The London Borough of Southwark (Waiting and loading restrictions) (Amendment No. *) Order 20***

As part of their Road Modernisation Plan, Transport for London (TfL) has identified improvements to the highway that would reduce bus journey times, at the College Road and Crystal Palace Road junction, within the Dulwich Wood ward in the London Borough of Southwark.

The proposed changes to this junction are designed to compliment the cycling and pedestrian improvements to Crystal Palace Parade, and surrounding areas (Fountain Drive / Sydenham Hill / Westwood Hill), which have been implemented in the recent months.

This scheme proposes to introduce to the junction of College Road and Crystal Palace Parade:

- a compulsory left turn lane and north-eastbound bus lane in Crystal Palace Parade
- a dedicated left-turning lane from Crystal Palace Parade to College Road
- prohibition of turns and manoeuvres to increase the ease of traffic flow in and around the junction
- widening of Crystal Palace Parade carriageway to accommodate the overall improvements
- amendment of and formalising existing bus lanes on Crystal Palace Parade up to the junction with College Road
- traffic signals upgrade to improve pedestrian and vehicle movements through the junction
- amending existing and adding new lengths of 'at any time' waiting restrictions around the junction

Link to decision: <http://moderngov.southwark.gov.uk/ieDecisionDetails.aspx?ID=6492>

For more information contact:-

Donovan Johnson

Project Manager

Highways – Transport Projects

Highways@southwark.gov.uk

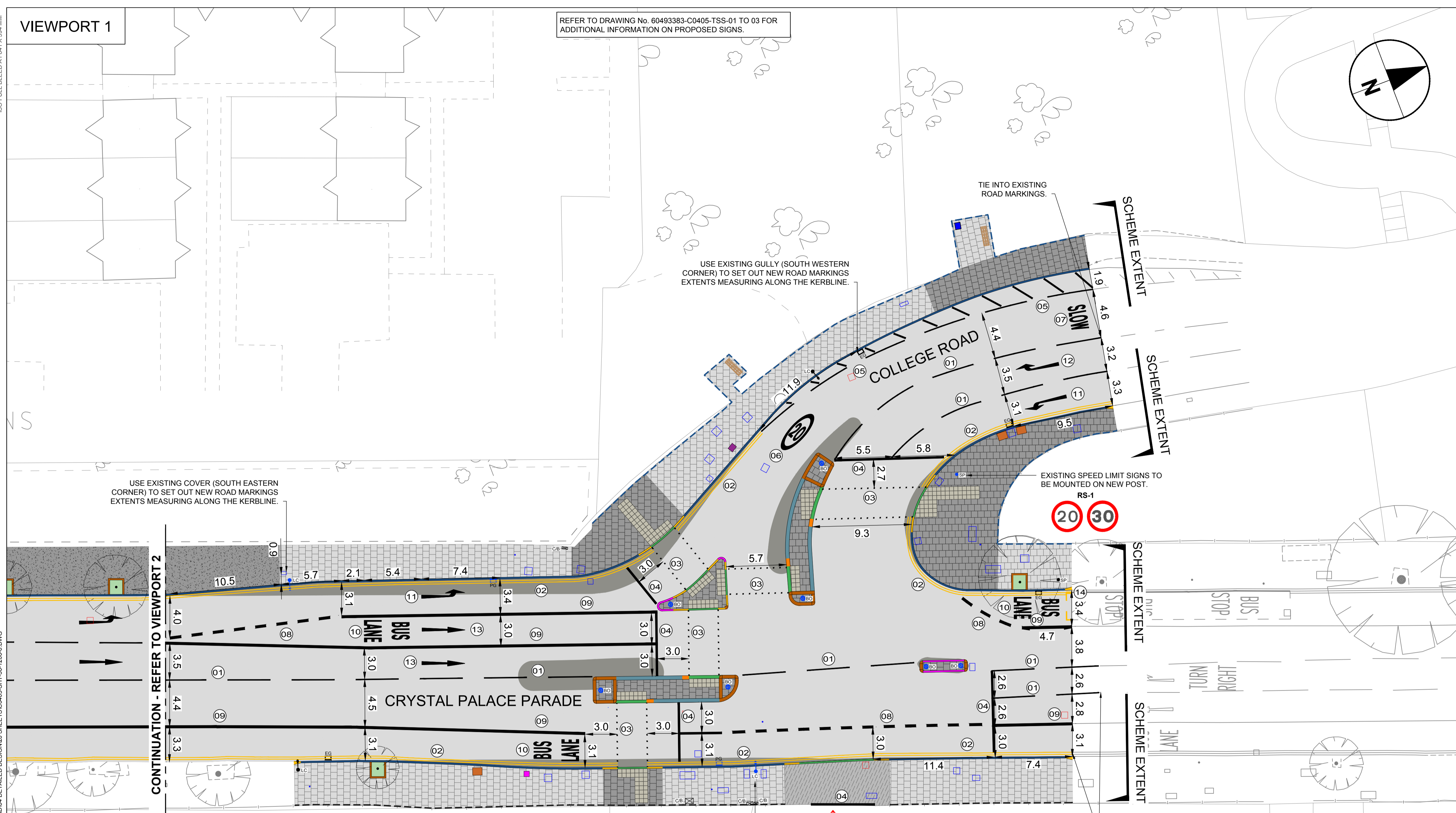
REFER TO DRAWING No. 60493383-C0405-TSS-01 TO 03 FOR ADDITIONAL INFORMATION ON PROPOSED SIGNS.

NOTES

- All dimensions are in metres unless otherwise stated.
- All items shown in grey are existing and are remain unless stated otherwise.
- The information shown on this drawing is based on a topographical survey undertaken by 'David J. Webb Surveys' in January 2017.
- This drawing is to be read in conjunction with all CONWAY AECOM drawings for job ref. 60493383-C0405 as listed on the scheme document issue sheet.
- This drawing is to be read in conjunction with LB Southwark Streetscape Design Guidance (SSDM) and all relevant contract clauses.
- Traffic management is to be in accordance with Chapter 8 of the Traffic Signs Manual and Safety at Street Works and Road Works.
- Contractor to maintain access to all properties and businesses at all times and agree alternative access arrangements with stakeholders.
- Contractor shall not park works vehicles or store materials in the vicinity of the retaining wall. Contractor shall ensure that all works next to the retaining wall are undertaken by hand digging to avoid damage to existing structure.
- Drawing to be printed in A1 for clarity.
- All road markings and signs are to be in accordance with the Traffic Signs Regulations & General Directions (TSRGD) 2016.
- All proportions and form of letters, numerals and other characters are to be in accordance with Schedule 17 of the TSRGD 2016.
- Double yellow lines to be offset a minimum of 250mm from edge of the kerb.
- All street furniture except lighting columns should be set a minimum of 450mm from the kerb face.

KEY

- New kerbs
- Road marking reference. Refer to Table 1 (Road Marking Schedule).
- Existing road sign and post.
- New road sign and post.
- Existing lamp column.
- Proposed lamp column location. To be confirmed by LBS Street Lighting Department.
- Double yellow lines.
- White thermoplastic road marking.
- White thermoplastic arrow marking.
- New bollards on traffic island. Refer to drawing 60493383-C0405-SD-05 detail H.
- Existing utility covers.
- Existing gully.
- Existing manhole.



USE EXISTING COVER (SOUTH EASTERN CORNER) TO SET OUT NEW ROAD MARKINGS EXTENTS MEASURING ALONG THE KERBLINE.

USE EXISTING GULLY (SOUTH WESTERN CORNER) TO SET OUT NEW ROAD MARKINGS EXTENTS MEASURING ALONG THE KERBLINE.

TIE INTO EXISTING ROAD MARKINGS.

EXISTING SPEED LIMIT SIGNS TO BE MOUNTED ON NEW POST.

TIE INTO EXISTING ROAD MARKINGS.

TV MAST COMPOUND PRIVATE ACCESS



EXISTING SIGNS TO BE MOUNTED ON RELOCATED LAMP COLUMN.

TABLE 1 - ROAD MARKING SCHEDULE (TSRGD 2016)

REF NO.	DWG NO.	MARK MM	GAP (MM)	WIDTH (MM)	TYPE	COLOUR	COMMENT
01	1004	4000	2000	100	INTERMITTENT	WHITE	HAZARD MARKING
02	1018.1	-	50	50	CONTINUOUS	PRIMROSE YELLOW	DOUBLE YELLOW LINES
03	1055.1	100	500	100	INTERMITTENT	WHITE	STUDS
04	1001	-	-	200	CONTINUOUS	WHITE	STOPLINE
05	1040.4	4000	2000	100	INTERMITTENT	WHITE	HATCHING
06	1065	4300	-	1500	SYMBOL	WHITE	MAXIMUM SPEED LIMIT '20MPH'
07	1024	1600	-	70	TEXT	WHITE	"SLOW" TEXT
08	1010	1000	1000	250	INTERMITTENT	WHITE	BUS LANE TAPER
09	1049A	-	-	250	CONTINUOUS	WHITE	BOUNDARY OF BUS LANE
10	1048	1600	1000	50	TEXT	WHITE	"BUS LANE" TEXT
11	1038	4000	-	750	SYMBOL	WHITE	LEFT TURN ARROW
12	1038	4000	-	750	SYMBOL	WHITE	RIGHT TURN ARROW
13	1038	4000	-	500	SYMBOL	WHITE	AHEAD ARROW
14	1025.1	1000	1000	100	INTERMITTENT	PRIMROSE YELLOW	BUS STOP CAGE MARKING
		-	-	200	CONTINUOUS	PRIMROSE YELLOW	BUS STOP CLEARWAY
		1600	1000	70	TEXT	PRIMROSE YELLOW	"BUS STOP" TEXT
15	1050	5550	-	3200	SYMBOL	WHITE	DIRECTION OF TRAFFIC

REV	DATE	REVISION DESCRIPTION / DETAILS	DRN BY	CHKD BY	APRVD BY



Shaping Southwark's Highways
160 TOOLEY STREET LONDON SE1P 5LX

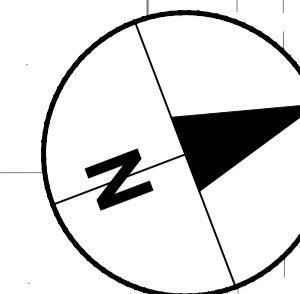
PROJECT: RMP COLLEGE ROAD JUNCTION

TITLE: SERIES 1200 ROAD MARKINGS & SIGNS SHEET 1 OF 2

CONTRACT NO:	60493383-C0405	DRAWN:	AEF
SCALE:	1 : 200 @ A1	DESIGNED:	AEF
DRAWING NO:	60493383-C0405-1200-01	CHECKED:	JE
DATE DRAWN:	18 OCTOBER 2017	APPROVED:	KC
DATE ISSUED:	16 NOVEMBER 2017	REV:	-

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REFER TO DRAWING No. 60493383-C0405-TSS-01 TO 03 FOR ADDITIONAL INFORMATION ON PROPOSED SIGNS.



LEGEND / NOTES

NOTES

- All dimensions are in metres unless otherwise stated.
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- The information shown on this drawing is based on a topographical survey undertaken by 'David J. Webb Surveys' in January 2017.
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- This drawing is to be read in conjunction with LB Southwark Streetscape Design Guidance (SSDM) and all relevant contract clauses.
- Traffic management is to be in accordance with Chapter 8 of the Traffic Signs Manual and Safety at Street Works and Road Works.
- Contractor to maintain access to all properties and businesses at all times and agree alternative access arrangements with stakeholders.
- Contractor shall not park works vehicles or store materials in the vicinity of the retaining wall. Contractor shall ensure that all works next to the retaining wall are undertaken by hand digging to avoid damage to existing structure.
- Drawing to be printed in A1 for clarity.
- All road markings and signs are to be in accordance with the Traffic Signs Regulations & General Directions (TSRGD) 2016.
- All proportions and form of letters, numerals and other characters are to be in accordance with Schedule 17 of the TSRGD 2016.
- Double yellow lines to be offset a minimum of 250mm from edge of the kerb.
- All street furniture except lighting columns should be set a minimum of 450mm from the kerb face.

KEY

- New kerbs
- Road marking reference. Refer to Table 1 (Road Marking Schedule).
- New road sign and post.
- Existing lamp column.
- Double yellow lines.
- White thermoplastic road marking.
- White thermoplastic arrow marking.
- Existing utility covers.
- Existing gully.
- Indicative location for bus stop shelter (back to kerb layout). To be installed by TfL bus infrastructure.
- Bus stop flag location.
- Existing manhole.

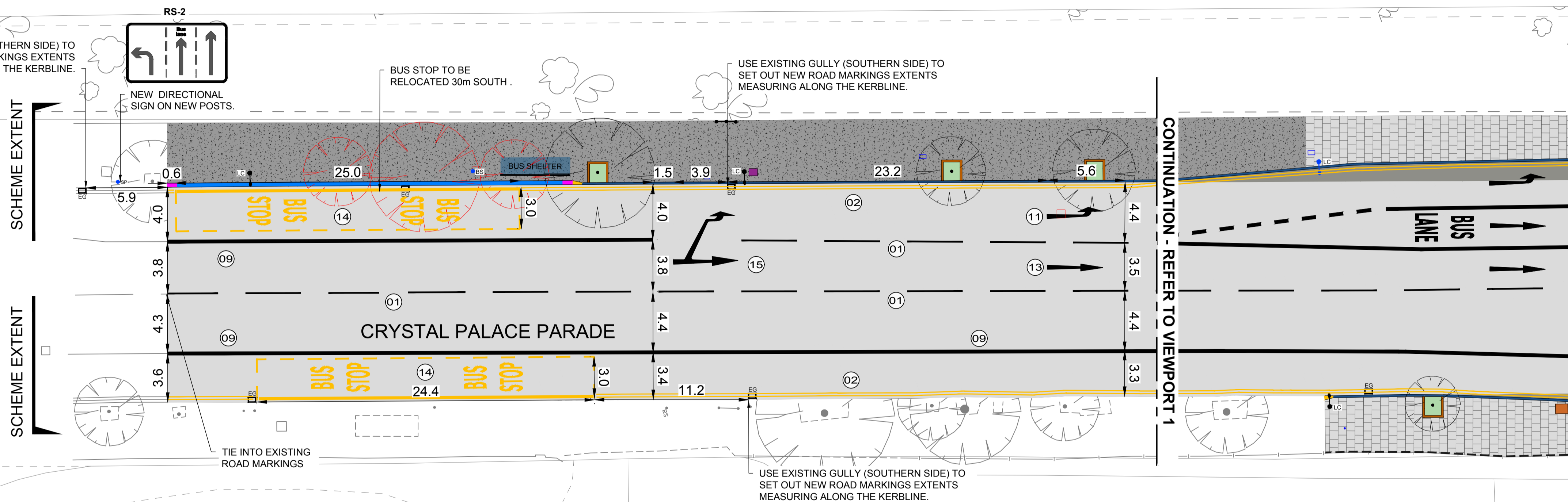


TABLE 1 - ROAD MARKING SCHEDULE (TSRGD 2016)

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03	1055.1	100	500	100	INTERMITTENT	WHITE	STUDS
04	1001	-	-	200	CONTINUOUS	WHITE	STOPLINE
05	1040.4	4000	2000	100	INTERMITTENT	WHITE	HATCHING
06	1065	4300	-	1500	SYMBOL	WHITE	MAXIMUM SPEED LIMIT '20MPH
07	1024	1600	-	70	TEXT	WHITE	"SLOW" TEXT
08	1010	1000	1000	250	INTERMITTENT	WHITE	BUS LANE TAPER
09	1049A	-	-	250	CONTINUOUS	WHITE	BOUNDARY OF BUS LANE
10	1048	1600	1000	50	TEXT	WHITE	"BUS LANE" TEXT
11	1038	4000	-	750	SYMBOL	WHITE	LEFT TURN ARROW
12	1038	4000	-	750	SYMBOL	WHITE	RIGHT TURN ARROW
13	1038	4000	-	500	SYMBOL	WHITE	AHEAD ARROW
14	1025.1	1000	1000	100	INTERMITTENT	PRIMROSE YELLOW	BUS STOP CAGE MARKING
		-	-	200	CONTINUOUS	PRIMROSE YELLOW	BUS STOP CLEARWAY
		1600	1000	70	TEXT	PRIMROSE YELLOW	"BUS STOP" TEXT
15	1050	5550	-	3200	SYMBOL	WHITE	DIRECTION OF TRAFFIC

REV	DATE	REVISION DESCRIPTION / DETAILS	DRN BY	CHKD BY	APRVD BY
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PROJECT: RMP COLLEGE ROAD JUNCTION

TITLE: SERIES 1200 ROAD MARKINGS & SIGNS SHEET 2 OF 2

CONTRACT NO:	60493383-C0405	DRAWN	AEF
SCALE:	1 : 200 @ A1	DESIGNED	AEF
DRAWING NO:	60493383-C0405-1200-02	CHECKED	JE
DATE DRAWN:	18 OCTOBER 2017	APPROVED	KC
DATE ISSUED:	16 NOVEMBER 2017	REV:	-

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THE COUNCIL OF THE LONDON BOROUGH OF SOUTHWARK

TRAFFIC MANAGEMENT ORDER

20** No. ***

The London Borough of Southwark (Bus priority) (Crystal Palace Parade) (No. 2)
Traffic Order 20**

Made: xx xxxxxxxx 20**

Coming into force: xx xxxxxxxx 20**

The council of the London Borough of Southwark, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of the Road Traffic Regulation Act 1984^a, as amended, and of all other powers thereunto enabling, hereby make the following Order:

Citation and commencement

1.1 This Order may be cited as the London Borough of Southwark (Bus priority) (Crystal Palace Parade) (No. 2) Traffic Order 20**, and shall come into force on xx xxxxxxxx 20**.

Interpretation

2.1 In this Order-

a reference to an Article or Schedule followed by a number is a reference to the Article of, or Schedule to, this Order so numbered;

causing includes permitting;

"carriageway" has the same meaning as in section 329(1) of the Highways Act 1980^b;

"vehicle" includes part of a vehicle;

"bus", "local bus", "pedal cycle", "taxi", "traffic lane", "contra-flow" and "with-flow lane" have the meanings given in Schedule 1 to the Traffic Signs Regulations and General Directions 2016^c;

"bus lane" means a traffic lane on a length of road which is specified in column (2) in the table in Schedule 1 and in respect of which traffic signs are in place indicating the controls specified in this Order;

"bus-only route" means an area of road which is specified in column (2) in the table in Schedule 2 and in respect of which traffic signs are in place indicating the controls specified in this Order;

^a 1984 c.27

^b 1980 c.66

^c SI 2016/362

"kerb-line" means the imaginary line which is the projection of the line formed by the edge of the carriageway of a street, and for the avoidance of doubt, any dimension in a specified street referred to in this Order which is measured from the kerb-line of another specified street, shall be construed as though measured from a point at the intersection of the two kerb-lines of those two streets;

"times of operation" means in relation to a bus lane specified in an item in column (2) of the table in Schedule 1, the times specified in column (3) of that item, and in relation to a bus-only route specified in an item in column (2) of the table in Schedule 2, the times specified in column (3) of that item; and

"traffic sign" means a sign of any size, type and colour prescribed and authorised under, or as having effect as though prescribed or authorised under section 64 of the Road Traffic Regulation Act 1984.

2.2 The Interpretation Act 1978^d applies to this Order as if it were an enactment.

Bus priority controls

- 3.1 Except as provided in Articles 4 and 5, no person shall cause a vehicle to be in a bus lane in a length of road specified in an item in column (2) of the table in Schedule 1 during the times specified in that item in column (3) of that item unless that vehicle is proceeding in the direction specified in column (4) of that item and is of a type specified in column (5) of that item.
- 3.2 Except as provided in Article 5, no person shall cause a vehicle to be in a bus-only route in an area of road specified in an item in column (2) of the table in Schedule 2 during the times specified in that column (3) of that item unless that vehicle is proceeding in the direction or directions specified in column (4) of that item and is of a type specified in column (5) of that item.

Exemptions - bus lanes

- 4.1 The controls specified in Article 3.1 do not apply so as to prevent a vehicle being in a bus lane if and for as long as necessary-
- (a) to enable a person to get on or off the vehicle; or
 - (b) to enable goods to be loaded on to or unloaded from the vehicle;
- if and in so far as the activity is not prohibited by the provisions of any other Order.
- 4.2 The controls specified in Article 3.1 do not apply in respect of a vehicle which is being used for a purpose which is within an exemption to a restriction or prohibition imposed by any other Order or which is permitted by any other Order.
- 4.3 The controls specified in Article 3.1 do not apply in respect of a vehicle crossing a bus lane to get to or from any road adjacent to the bus lane or any vehicular access to premises adjacent to the bus lane.

^d 1978 c.30

Exemptions - bus lanes and bus-only routes

- 5.1 The controls specified in Article 3.1 do not apply in respect of a vehicle being in a bus lane if the vehicle is being used:-
- (a) in the service of a local authority for the purpose of collecting refuse;
 - (b) for the purposes of the provision of a universal postal service (as defined in the Postal Services Act 2011^e) provided the vehicle bears the livery of a universal service provider (as defined in the aforementioned Act);
 - (c) in connection with the removal of any obstruction to traffic;
 - (d) in connection with the maintenance, improvement or reconstruction of any road;
 - (e) in connection with the maintenance, improvement or construction of any street furniture including bus stop infrastructure;
 - (f) in connection with any building operation or demolition;
 - (g) in connection with the laying, erection, alteration or repair in or near the bus lane of any sewer or of any main, pipe or apparatus for the supply of gas, water or electricity, or of any electronic communications network (as defined in section 32 of the Communications Act 2003^f), providing the vehicle cannot be used for that purpose without being in the bus lane or the bus-only route and in so far as the activity is not prohibited by the provisions of any other Order.
- 5.2 The controls specified in Article 3.1 do not apply in respect of a vehicle being in a bus lane:
- (a) so as to avoid an accident;
 - (b) to allow a person to get or give help in consequence of an accident or emergency or otherwise take action for public safety;
 - (c) to such an extent and for such a time as is needed to comply with any requirement resulting from works or an emergency in any part of the road adjoining the bus lane or bus-only route;
 - (d) if it is being used for ambulance, fire brigade or police purposes.
- 5.3 The controls specified in Articles 3.1 and 3.2 do not apply in respect of anything done with the permission or at the direction of a police constable in uniform or a civil enforcement officer.

Dated this xxxxxxxx day of xxxxxxxx 20**

[signature here]

NICKY COSTIN
Parking and Network Management Business Unit Manager
Regulatory Services

^e 2011 c.5
^f 2003 c.21

SCHEDULE 1 - BUS LANES

(1) Item	(2) Lengths of road	(3) Times of operation	(4) Direction of travel and type of bus lane	(5) Type of permitted vehicle
1.	CRYSTAL PALACE PARADE the north-west side, from the boundary of the London Borough of Bromley to a point 80 metres south-west of the south-western kerb-line of College Road.	At any time	North-eastbound with-flow lane	Bus Pedal cycle Taxi
2.	CRYSTAL PALACE PARADE all that part of the north-eastbound carriageway as is bounded on the north-west by a line drawn 3.4 metres south-east of and parallel to the north-western kerb-line of Crystal Palace Parade and marked by a traffic sign and bounded on the south-east by a line drawn 6.4 metres south-east of and parallel to the north-western kerb-line of Crystal Palace Parade and marked by a traffic sign, from a point 27 metres south-west of a point opposite the south-western kerb-line of College Road to a point 2 metres north-east of a point opposite the south-western kerb-line of College Road.	At any time	North-eastbound with-flow lane	Bus Pedal cycle Taxi

SCHEDULE 2 - BUS-ONLY ROUTES

<i>(1) Item</i>	<i>(2) Lengths of road</i>	<i>(3) Times of operation</i>	<i>(4) Direction of travel</i>	<i>(5) Type of permitted vehicle</i>
No item.				

EXPLANATORY NOTE

(This note is not part of the Order)

This Order:-

- (i) introduces a new north-eastbound bus lane for the use of buses, pedal cycles and taxis only marked out in the centre of the north-eastbound carriageway of Crystal Palace Parade at its junction with College Road; and
- (ii) formalises an existing north-eastbound bus lane for the use of buses, pedal cycles and taxis only in Crystal Palace Parade, between the boundary of the London Borough of Bromley and a point 80 metres south-west of its junction with College Road;

in the London Borough of Southwark.



THE COUNCIL OF THE LONDON BOROUGH OF SOUTHWARK

TRAFFIC MANAGEMENT ORDER

20** No. 0**

The London Borough of Southwark (Prescribed routes)
(Crystal Palace Parade) (No. 2) Traffic Order 20**

Made: xx xxxxxxxx 20**

Coming into force: xx xxxxxxxx 20**

The council of the London Borough of Southwark, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of the Road Traffic Regulation Act 1984^a, as amended, and of all other powers thereunto enabling, hereby make the following Order:

Citation and commencement

1.1 This Order may be cited as the London Borough of Southwark (Prescribed routes) (Crystal Palace Parade) (No. 2) Traffic Order 20**, and shall come into force on xx xxxxxxxx 20**.

Interpretation

2.1 In this Order:-

causing includes permitting;

“bus lane” “carriageway” and “traffic lane” have the same meaning as in Schedule 1 to the Traffic Signs Regulations and General Directions 2016^b;

“College Road central island site” means a traffic island with a signalized pedestrian crossing facility constructed in College Road at its junction with Crystal Palace Parade, comprising all that part of the centre of the carriageway as is bounded on its north-eastern edge by a kerb constructed 9.3 metres south-west of and parallel to the north-eastern kerb-line of College Road and on its south-western edge by a kerb constructed 8.2 metres north-east of and parallel to the south-western kerb-line of College Road, measured at the centre of the traffic island, as extends from a point 1 metre south-east of a point opposite the north-western kerb-line of Crystal Palace Parade to a point 13 metres north-west of a point opposite the north-western kerb-line of Crystal Palace Parade, measured on the north-east side of the junction of College Road and Crystal Palace Parade;

“College Road and Crystal Palace Parade island site” means a traffic island with a signalized pedestrian crossing facility constructed at the junction of College Road and Crystal Palace Parade, comprising all that part of the north-westbound carriageway of College Road and the north-eastbound carriageway of Crystal Palace Parade as is bounded on its north-eastern edge by a kerb constructed 5.7 metres south-west of and parallel to the south-western edge of the College Road central island site, on its south-eastern edge by a kerb constructed 3.4 metres south-east of and parallel to the north-western kerb-line of Crystal Palace Parade, measured on the south-west side of the junction of College Road and Crystal Palace Parade, and bounded

^a 1984 c.27

^b SI 2016/362

on its south-western edge by a kerb constructed 5 metres north-east of and parallel to the south-western kerb-line of College Road, as extends in Crystal Palace Parade from a point 2 metres north-east of a point opposite the south-western kerb-line of College Road to a point 9 metres north-east of a point opposite the south-western kerb-line of College Road, and extends in College Road from a point 3.4 metres south-east of a point opposite the north-western kerb-line of Crystal Palace Parade to a point 1.6 metres north-west of a point opposite the north-western kerb-line of Crystal Palace Parade;

“Crystal Palace Parade left turn traffic lane” means a nearside traffic lane comprising all that part of the north-eastbound carriageway of Crystal Palace Parade as is bounded on the north-west by the north-western kerb-line of Crystal Palace Parade and on the south-east by a traffic sign indicating the north-western edge of the Crystal Palace Parade north-eastbound bus lane, as extends from a point 27 metres south-west of the south-western kerb-line of College Road to the south-western end of the College Road and Crystal Palace Parade island site, and which is marked by a traffic sign as being for the use of vehicles turning left;

“Crystal Palace Parade north-eastbound bus lane” means the carriageway central bus lane referred to in item 2 of Schedule 1 to the London Borough of Southwark (Bus Priority) (Crystal Palace Parade) (No. 2) Traffic Order 20**^c;

“Crystal Palace Parade north-eastbound offside traffic lane” means an offside traffic lane comprising all that part of the north-eastbound carriageway of Crystal Palace Parade as is bounded on the north-west by a traffic sign indicating the south-eastern edge of the Crystal Palace Parade north-eastbound bus lane and on the south-east by a traffic sign indicating the north-western edge of the south-westbound carriageway of Crystal Palace Parade, as extends from a point 27 metres south-west of a point opposite the south-western kerb-line of College Road to a point opposite the south-western end of the College Road and Crystal Palace Parade island site, and which is marked by a traffic sign as being for the use of vehicles proceeding ahead;

“enactment” means any enactment, whether public general or local, and includes any order, byelaw, rule, regulation, scheme or other instrument having effect by virtue of an enactment;

“kerb-line” means the imaginary line which is the projection of the line formed by the edge of the carriageway of a street, and for the avoidance of doubt, any dimension in a specified street referred to in this Order which is measured from the kerb-line of another specified street, shall be construed as though measured from a point at the intersection of the two kerb-lines of those two streets;

“traffic sign” means a sign of any size, type and colour prescribed and authorised under, or having effect as though prescribed or authorised under section 64 of the Road Traffic Regulation Act 1984.

- 2.2 Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.
- 2.3 Any reference in the Order to a named road, street or part thereof shall be construed as a reference to a road, street or part thereof having that name which lies within the London Borough of Southwark.

^c LBS 20**/xxx

Prescribed routes

- 3.1 Every person causing any vehicle to proceed in a north-eastbound direction in the Crystal Palace Parade left turn traffic lane shall, upon reaching its junction with College Road, cause that vehicle to turn left into College Road.
- 3.2 Every person causing any vehicle to proceed in a north-eastbound direction in the Crystal Palace Parade north-eastbound bus lane shall, upon reaching its junction with College Road, cause that vehicle to proceed ahead into that part of Crystal Palace Parade which lies to the north-east of its junction with College Road.
- 3.3 Every person causing any vehicle to proceed in a north-eastbound direction in the Crystal Palace Parade north-eastbound offside traffic lane shall, upon reaching its junction with College Road, cause that vehicle to proceed ahead into that part of Crystal Palace Parade which lies to the north-east of its junction with College Road.
- 3.4 No person causing any vehicle to proceed in a north-eastbound direction in the Crystal Palace Parade north-eastbound bus lane shall, upon reaching its junction with College Road, cause that vehicle to turn right into the vehicular access to the Crystal Palace transmitting station, currently known as 'Arqiva Crystal Palace'.
- 3.5 No person causing any vehicle to proceed in a north-eastbound direction in the Crystal Palace Parade north-eastbound offside traffic lane shall, upon reaching its junction with College Road, cause that vehicle to turn right into the vehicular access to the Crystal Palace transmitting station, currently known as 'Arqiva Crystal Palace'.
- 3.6 No person causing any vehicle to proceed in a south-eastbound direction in that part of the carriageway of College Road which lies to the north-east of the College Road central island site shall, upon reaching the south-eastern extremity of that island site, turn that vehicle so as to face in the opposite direction to that in which it was proceeding, that is, so as to enter into that part of the carriageway of College Road which lies to the north-east of the College Road and Crystal Palace Parade island site and to the south-west of College Road central island site.

Exemptions

- 4.1 Nothing in Articles 3.1 to 3.6 inclusive above shall apply in relation to:-
 - (a) anything done with the permission or at the direction of a police constable in uniform;
 - (b) any vehicle being used for ambulance, fire brigade or police purposes in an emergency;
 - (c) any person who causes any vehicle to proceed in accordance with any restriction or requirement indicated by traffic signs placed on the highway pursuant to section 66 or section 67 of the Road Traffic Regulation Act 1984.

Dated this xxxxxxxx day of xxxxxxxx 20**

[signature here]

NICKY COSTIN
Parking and Network Management Business Unit Manager
Regulatory Services

EXPLANATORY NOTE

(This note is not part of the Order)

This Order:-

- (i) provides a compulsory left turn lane for north-eastbound vehicles in Crystal Palace Parade on the approach to its junction with College Road;
- (ii) provides compulsory ahead only north-eastbound traffic working for north-eastbound vehicles using the carriageway central bus lane and offside traffic lane in Crystal Palace Parade on the approach to its junction with College Road;
- (iii) prohibits north-eastbound vehicles using the carriageway central bus lane and offside traffic lane in Crystal Palace Parade on the approach to its junction with College Road from turning right into the vehicular access to the Crystal Palace transmitting station, currently known as 'Arqiva Crystal Palace'; and
- (iv) prohibits south-eastbound vehicles in College Road from undertaking a U-turn manoeuvre at the south-eastern end of the central traffic island in College Road at its junction with Crystal Palace Parade;

in the London Borough of Southwark.



THE COUNCIL OF THE LONDON BOROUGH OF SOUTHWARK

TRAFFIC MANAGEMENT ORDER

20** No. 0**

The London Borough of Southwark (Waiting and loading restrictions)
(Amendment No. **) Order 20**

Made: xx xxxxxxxx 20**

Coming into force: xx xxxxxxxx 20**

The council of the London Borough of Southwark, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of the Road Traffic Regulation Act 1984^a, as amended, and of all other powers thereunto enabling, hereby make the following Order:

Citation and commencement

- 1.1 This Order may be cited as the London Borough of Southwark (Waiting and loading restrictions) (Amendment No. **) 20**, and shall come into force on xx xxxxxxxx 20**.

Interpretation

- 2.1 In this Order:-

"council" means the council of the London Borough of Southwark;

"enactment" means any enactment, whether public general or local, and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment; and

"the Order of 2015" means the London Borough of Southwark (Waiting and loading restrictions) Consolidation Order 2015^b - as amended.

- 2.2 Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.
- 2.3 Unless the context otherwise requires, any expression used in this Order which is also used in the Order of 2015 shall have the same meaning as in that Order.

^a 1984 c.27

^b LBS 2015/082

Amendment of the Order of 2015

- 3.1 Without prejudice to the validity of anything done, or to any liability incurred in respect of any act or omission before the coming into force of this Order, the Order of 2015 shall have effect as though for the items numbered 214 and 1149 in Schedule 1 to that Order there were substituted the item similarly numbered and set out in columns 1, 2 and 3 of the Schedule to this Order.

Dated this xxxxxxxx day of xxxxxxxx 20**

[signature here]

NICKY COSTIN
Parking and Network Management Business Unit Manager
Regulatory Services

SCHEDULE – WAITING RESTRICTIONS
(*Substitution in Schedule 1 to the Order of 2015*)

<i>Item No. (1)</i>	<i>Street (2)</i>	<i>Prescribed hours (3)</i>
214.	COLLEGE ROAD	
	(a) the east, north-east and south-east side	
	(i) between its junction with Dulwich Village and the northern kerb-line of the vehicular access to Dulwich Park at Old College Gate;	At any time
	(ii) between a point 43 metres south of the southern kerb-line of the vehicular access to Dulwich Park at Old College Gate and the common boundary of Nos. 19 and 21 College Road;	At any time
	(iii) between a point 2 metres north of the common boundary of Bell House, No. 29 College Road and No. 31 College Road and a point 5 metres south of the common boundary of Nos. 31 and 33 College Road;	At any time
	(iv) between a point 7.5 metres north of the northern kerb-line of Frank Dixon Way and a point 7.5 metres south of the southern kerb-line of Frank Dixon Way;	At any time
	(v) between the south-western kerb-line of Fountain Drive and a point 7.5 metres south of that kerb-line;	At any time
	(vi) between a point 23 metres north-west of a point opposite the northern kerb-line of Dulwich Wood Park and a point 13 metres south-east of a point opposite the northern kerb-line of Dulwich Wood Park;	At any time
	(vii) between the north-western kerb-line of Crystal Palace Parade and a point 29 metres north-west and north of that kerb-line, measured in an anti-clockwise direction from the north-western kerb-line of Crystal Palace Parade;	At any time
	(b) the west and south-west side	
	(i) between its junction with Dulwich Village and a point 15.5 metres south of the southern kerb-line of College Gardens at its northernmost junction with College Road;	At any time
	(ii) between a point 7.5 metres north of the northern kerb-line of kerb-line of College Gardens at its southernmost junction with College Road and a point 40 metres south of the southern kerb-line of College Gardens at its southernmost junction with College Road;	At any time
	(iii) between a point 2 metres north of the northern kerb-line of Kingswood Drive and a point 15.5 metres south of the southern kerb-line of Kingswood Drive;	At any time

SCHEDULE – WAITING RESTRICTIONS

<i>Item No. (1)</i>	<i>Street (2)</i>	<i>Prescribed hours (3)</i>
	(iv) between a point 8 metres south of the common boundary of Nos. 126 and 128 College Road and a point 6 metres south of the common boundary of Nos. 128 and 130 College Road;	At any time
	(v) between the northern kerb-line of Dulwich Wood Park and a point 13 metres north-west of that kerb-line;	At any time
	(vi) between the north-western kerb-line of Crystal Palace Parade and a point 18 metres north-west of that kerb-line.	At any time
1149.	CRYSTAL PALACE PARADE	
	the north-west side, between the boundary of the London Borough of Bromley and the south-western kerb-line of Sydenham Hill.	At any time

EXPLANATORY NOTE

(This note is not part of the Order)

This Order introduces new lengths of 'at any time' waiting restrictions in College Road and Crystal Palace Parade in the London Borough of Southwark.